



**Planning Policy Manual – Part 1**

**Section 6.2**  
**Development Guidelines**  
**James, William, Roe and Lake Street Block**



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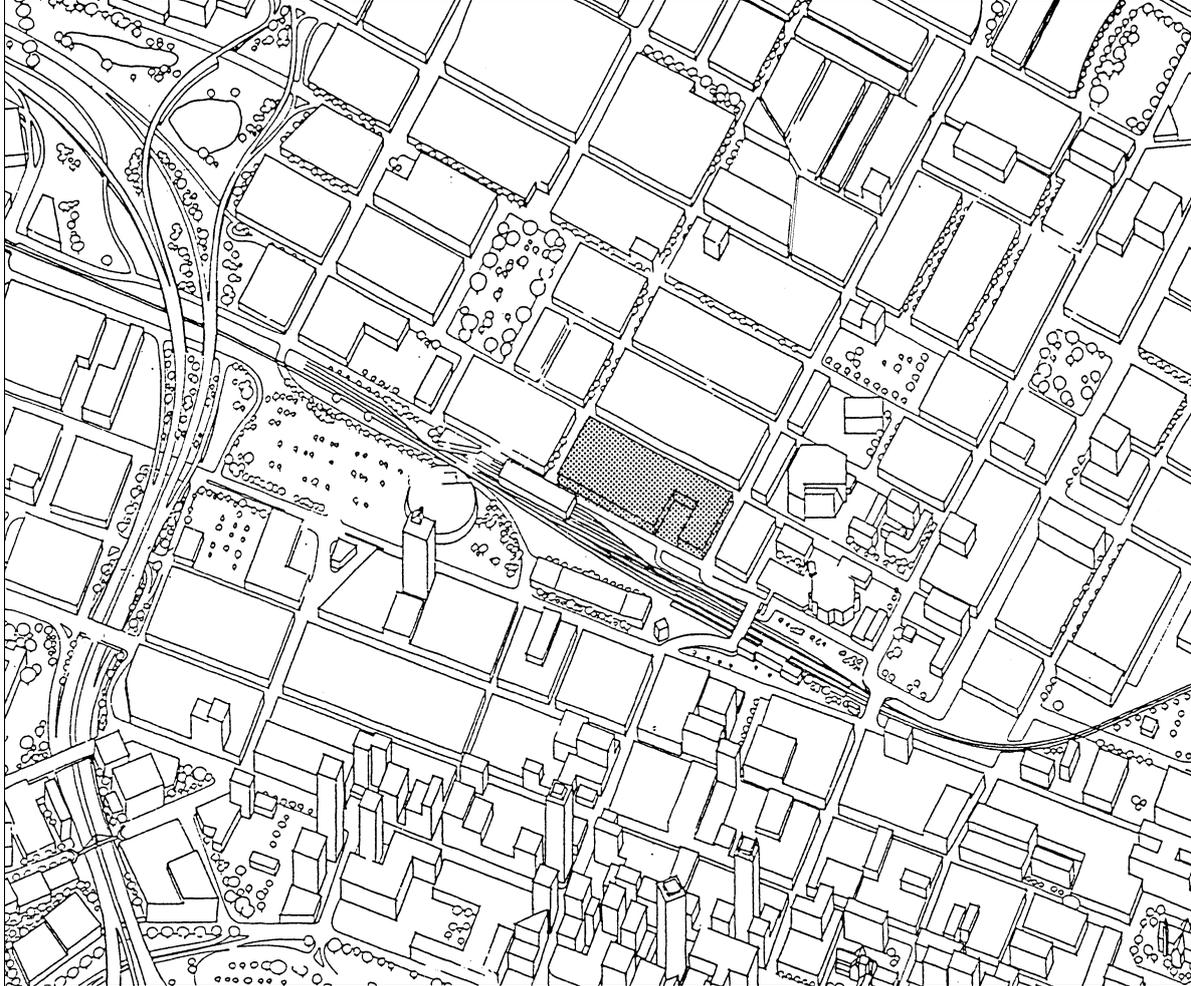


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## LOCATION PLAN



## 1. INTRODUCTION

This street block bounded by William, Roe, Lake and James Streets in the heart of Northbridge contains numerous sites with the potential for redevelopment.

The purpose of this document is to provide guidance to developers. The document provides a succinct description of the local government’s overall planning framework as it applies to this particular street block and immediate environs.

Some of the statements in this document (from the Scheme Text) are statutory requirements. Other provisions contained in the guidelines are non-statutory such as those extracted from the Northbridge Study Report (policy adopted by local government) or those evolved through examination of the street block. The guidelines are flexible and are intended to encourage positive action in accordance with the overall desired framework for this part of the city.

As intimated, the basis for these guidelines is contained within a number of documents. It is not intended that this document supersede these other documents but, rather, act as a distillation of the many policies and requirements contained within these and various other documents and studies.

## 2. STREET BLOCK OBJECTIVES

To achieve the desired character for Northbridge as described in the Northbridge Study Report the following objectives have been established.

- (1) Maintain diversity of activity in Northbridge.
- (2) Increase the opportunities for people to live and work in Northbridge.
- (3) Use planting and landscaping to create a unique character and quality for Northbridge.
- (4) Create attractive and humane public spaces in Northbridge.
- (5) Encourage public art in Northbridge to enliven the built environment and give expression to the cultural diversity of the area.
- (6) Encourage al fresco dining facilities that are attractive and safe.
- (7) Achieve a cohesive built environment where a diversity of Northbridge activities can be accommodated.
- (8) Ensure that Northbridge continues to be a place where people are able to move about comfortably on foot.
- (9) Ensure that the movement of vehicles within Northbridge is not to the detriment of the positive attributes of the area.
- (10) Ensure that the provision of car parking is not to the detriment of the positive attributes of Northbridge.
- (11) Build a strong and diverse community in Northbridge.
- (12) Ensure that Northbridge is promoted as an attractive destination amongst the local population, interstate and overseas visitors to Perth.



In relation to this street block the following particular objectives are also relevant.

- (1) To provide a range of activities having regard to its prime location in the entertainment/commercial area of Northbridge and its proximity to the cultural centre, the City Centre and public transport facilities;
- (2) To ensure that development, is of a scale and character which is complementary to existing traditional development and is suitable for the range of activities considered desirable in Northbridge;
- (3) To promote a quality and convenient pedestrian movement network at ground level.

### 3. LAND USE

#### 3.1 Appropriate Uses

As part of the premier entertainment area, the street block should attract a wide range of uses which provide visual interest and activity at street level during the day and night. These uses will, in the main but not exclusively, relate to entertainment, recreation, retail or cultural activities.

Diversity of activity is highly desirable. This is particularly the case along William, James and Lake Streets. While the above uses should be visually predominant at street level, there is an opportunity for a wide range of other uses (such as offices above ground floor) to be accommodated within this street block.

#### 3.2 Temporary Use

Whatever interim use is made of a site it is preferable that existing buildings are retained to provide an interesting and appropriate streetscape until the redevelopment can proceed.

#### 3.3 Al Fresco Dining

Al fresco dining is encouraged along James and Lake Streets. The local government has adopted a policy in this regard. The policy establishes the procedural matters in relation to obtaining a licence. The policy also sets down specific requirements in terms of location and standards for al fresco dining facilities.

A copy of these requirements can be obtained from the local government.



## 4. MOVEMENT NETWORKS AND PARKING

### 4.1 Pedestrians

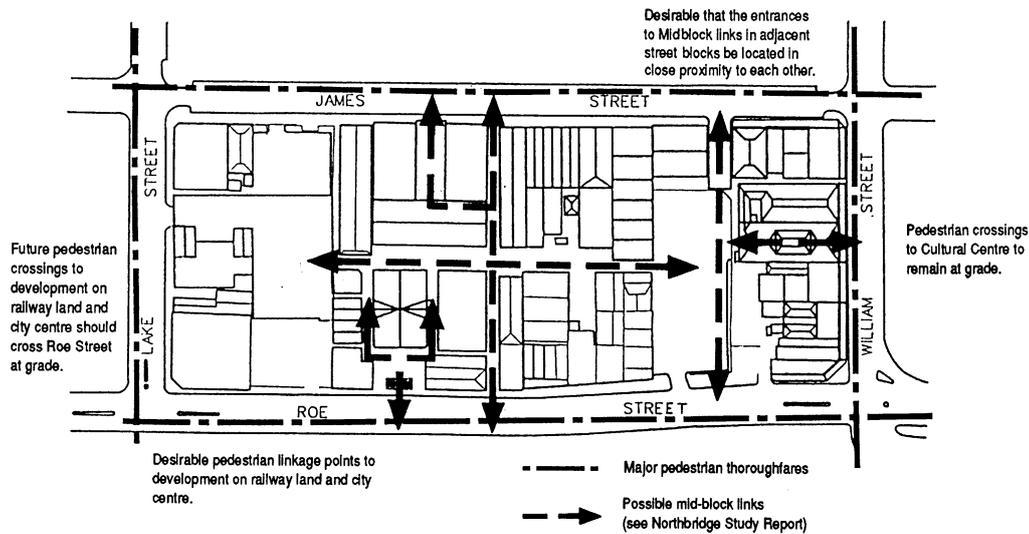
The growth in popularity of Northbridge as an eating and entertainment area will mean an increasingly high level of pedestrian movement. The footpath system, particularly in James and Lake Street, will invariably need to accommodate al fresco dining. As a consequence of this popularity and outdoor dining use the points of pedestrian/vehicle conflict will need to be minimised.

This is a long street block, however, a number of laneways linking Roe to James Street exist. Developers will be encouraged to create attractive mid-block pedestrian links from Roe to James Street. The location of these links should have regard to the wider pedestrian network and, in particular, the pedestrian linkages to adjoining street blocks. The street footpaths are the main pedestrian routes. The mid block links should not be developed at the expense of the main routes.

It is of paramount importance that all pedestrian movements remain at street level. The relationship between pedestrians in the street and activities at street level is very important to Northbridge.

Development adjacent to footpaths and other pedestrian areas should provide an attractive and interesting frontage to the route. Blank walls, exhaust fans, mechanical equipment and visible car parking and vehicle servicing areas should be avoided adjacent to pedestrian ways.

#### Pedestrian Network



Pedestrian Network

### 4.2 Tenant Parking

In this street block the City Planning Scheme does not require the provision of on-site parking facilities. The State Government’s Perth Parking Policy (as amended) places a pedestrian/environmental priority on certain streets in the city. James Street and William Street are considered to be of category 2 priority and the number of parking bays permitted depends on the type of access. Prospective developers should refer to the Perth Parking Policy (as amended) for detailed information.

Where tenant parking is provided, provision must also be made for delivery vehicles and visitor parking, unless it can be established that suitable alternative facilities are available (for example



public loading dock, public car park). These facilities should include provision for taxi and coach requirements in hotels.

### 4.3 Public Parking

The street block is located within the Short Stay Public Parking Zone.

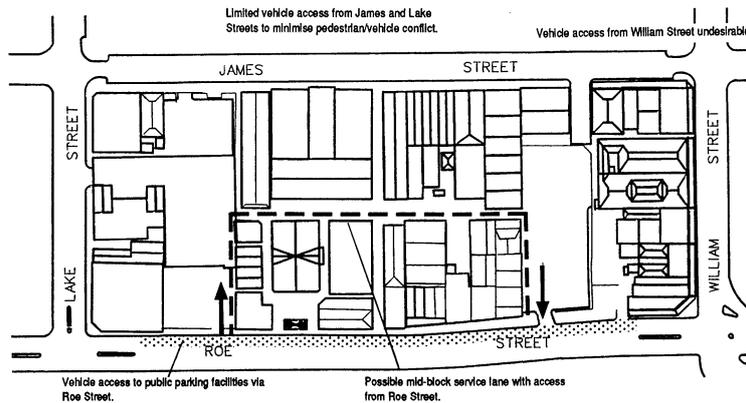
With regard to short stay public car parks such facilities may be permitted subject to acceptable traffic and environment conditions being achieved. Access to public car parking facilities should be restricted to Roe Street. This access should be achieved in a way that minimises the pedestrian/vehicle conflict.

### 4.4 Vehicle Access

Vehicle access to developments should be designed in a way which minimises the potential pedestrian vehicle conflict. Preference is, therefore, given to vehicle access via Roe Street. Porte cochere facilities for hotel and reception type developments will not be permitted. Passenger set down/pick up facilities will be given consideration on street.

On-site loading facilities should be provided. Opportunities to rationalise servicing exist by establishing a mid block service lane with access from Roe Street. This is intended to reduce the need for on-street servicing, especially in James Street.

#### Vehicle Access



Vehicle Access

## 5. STREETScape AND BUILT FORM CONSIDERATIONS

### 5.1 Northbridge

Pages 13-35 of the Northbridge Study Report set down the general objectives and policy proposals for the whole of Northbridge. There are comments and policies contained within those pages which provide a very clear expression of the preferred streetscape and built form within this street block. Of particular importance is the section on Built Form which makes reference to the built form guidelines in Appendix 1.

### 5.2 Site Area

The amalgamation of sites to create large site development areas will be discouraged. It is not intended to interfere with the rationalisation of property boundaries, however, the design of developments will be carefully considered in the light of the built form guidelines.

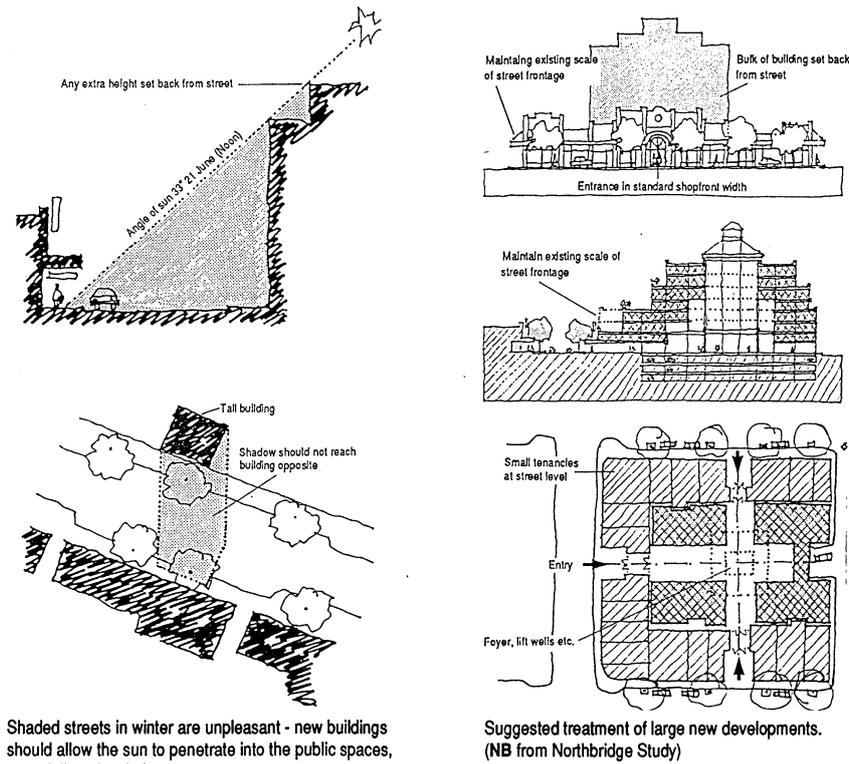


### 5.3 Plot Ratio

Refer to the Plot Ratio and Bonus Plot Ratio Plans and clauses 27, 28 and 30 contained within City Planning Scheme No. 2.

### 5.4 Heights of Buildings

The Scheme establishes a height control for all buildings.



Shaded streets in winter are unpleasant - new buildings should allow the sun to penetrate into the public spaces,

Notwithstanding these requirements any development should have regard to the desired character for Northbridge and the extreme importance of the streetscape as set down in the attached guidelines (Appendix 1).

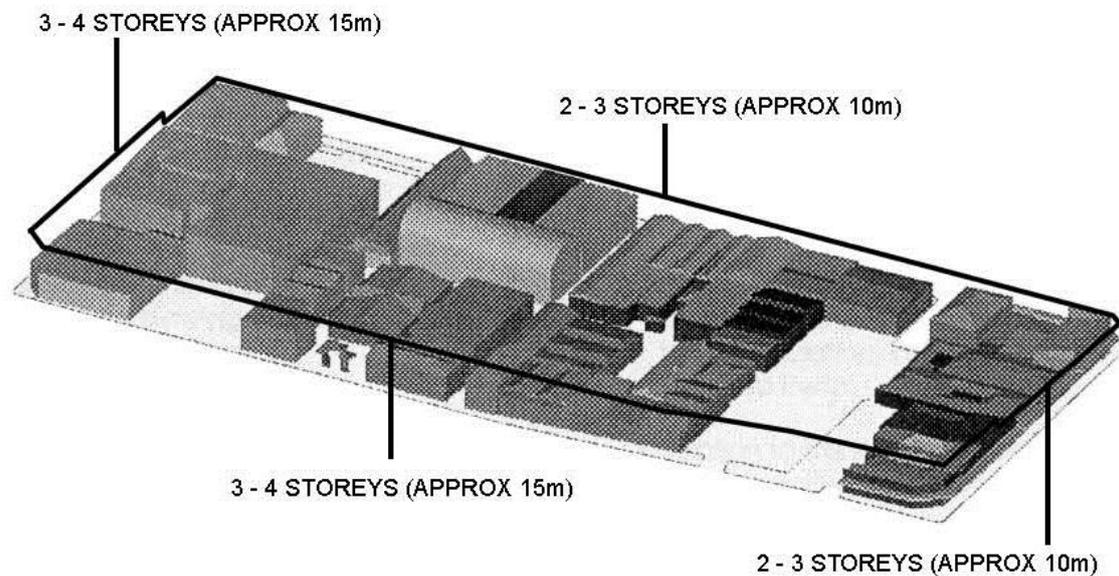
For any development incorporating higher rise buildings (typically when the height is greater than 10 storeys but may be required below this height in some circumstances), wind tunnel tests shall be provided in accordance with the Applications Policy to show that there would be no adverse wind impacts.

### 5.5 Frontage Height

The heights of buildings fronting the street should be in accordance with the Street Building Height and Setback Plan.

It is characteristic in Northbridge for buildings on the corners of street blocks to provide landmark elements. This has been achieved generally through the provision of a tower or spire like structure on the street corner of the development. Features of this kind may be appropriate on the corners of this street block.

### Frontage Height



### 5.6 Setbacks

Buildings should be setback from the street and side and rear lot boundaries in accordance with Street Building Height and Setback Plan, the Maximum Building Height Plan and the Building Heights and Setback Policy.

### 5.7 Awnings

Verandahs and awnings are an important characteristic of the streetscape in Northbridge and should be provided along all street frontages. Awnings can add value to the pedestrian amenity as well as providing scope for individuality for the occupier of the premises. To ensure the effectiveness of the awnings it is important that there is continuity along the street.

### 5.8 Staging of Development

If redevelopment of property within the street block is to occur in several stages, it is essential that each stage be physically and aesthetically complete in itself. The developer should give consideration to and advice on how it is proposed to minimise disruption to the street and adjoining premises.

### 5.9 Demolition and Vacant Land

Existing buildings should not be demolished until approval for new development is given and it is certain that new development will commence.

### 5.10 Hoardings and Gantries

Northbridge is a high volume pedestrian area. The standard of design, materials and finish of hoardings and gantries should be high to ensure pedestrian safety and amenity. Prospective developers should discuss these requirements with the local government officers at an early stage of a development.



## APPENDIX 1

### BUILT FORM GUIDELINES

The Northbridge Study promotes the retention and enhancement of many of the physical characteristics and activities already present in Northbridge. The Study also recognises and encourages the inevitable redevelopment facing the area.

The Built Form Guidelines are intended to act as a positive catalyst to promoting design compatibility and coherence between new developments and the existing built fabric of Northbridge. Therefore the Guidelines address issues of architectural and urban design quality.

The Guidelines are not rigid rules but design principles and characteristics to be adopted by architects and developers to create new buildings (or redevelop existing building stock) that will contribute to the special character of Northbridge.

In conjunction with the existing zoning and the other development controls (including plot ratio, building height and setbacks etc.) the Guidelines will be used by the local government to establish design parameters for new development proposals and to evaluate designs submitted for approval.

The design of new buildings should be responsive to the existing neighbouring and adjacent buildings, particularly when the existing buildings exhibit desirable “Northbridge characteristics”. Every new development will be expected to contribute to the creation of quality public space (refer to pages 21-22 of the Study).

The older buildings in Northbridge have some basic design characteristics that help create visual richness and variety.

The older buildings are more easily adapted to a wide variety of uses such as restaurants, cafes, night-clubs, offices, studios and warehouses, and the facades and form can reflect the individuality so important to the Northbridge experience.

Unfortunately, many of the newer developments have overlooked these basic urban design principles and consequently there is a loss of streetscape quality and urban character. Many of the newer developments do not allow separate tenants to express the individual images of their businesses and also fail to visually and physically integrate with adjoining developments to form homogenous public space.

When designing new buildings consideration should be given to the following:-

#### Urban Context

Design individual buildings as components in an urban streetscape. Each development should visually relate to its immediate neighbours whilst also contributing to the context and character of its general location.

The composition and massing of any development must address issues of urban context, form, scale, and visual character. This is especially the case with corner sites or prominent sites (e.g. around Russell Square), or sites that are easily seen from prominent vantage points (e.g. Cultural Centre, Horseshoe Bridge, Mitchell Freeway etc.).

#### Viewing distances

Consider the composition and detailing of a building from a variety of viewing distances and vantage points. Consider also the number of people viewing the building for what length of time and when



(day or night). These principles are particularly important for large or prominent developments. There are many good examples of older buildings in Northbridge which respected these principles.

### **Detail**

Visual richness and variety are characteristic of many older buildings in Northbridge. New buildings should incorporate a variety of materials and finishes that will either match or contrast with adjoining buildings and add to the variety and richness of the group of buildings of which they are part. It is not expected that this will necessarily entail reproducing features and details of older buildings.

Avoid large areas of monotonous, sheer materials such as polished -granite and curtain wall glazing. Large areas of a single colour or planar finish (e.g. painted render, metal cladding etc.) should also be avoided.

### **Hierarchy of Detail**

In order to design buildings that are visually interesting to the pedestrian and to a lesser extent people in vehicles, the ground level should contain the richer and more intricate design details. Unusual or unexpected details will add to the visual richness of Northbridge.

### **Shelter for Pedestrians**

Generally awnings and canopies are preferred to colonnading which results in the activities being set back from the street.

### **Adaptability**

Design commercial developments so that they can be easily modified by future occupants. Shop fronts (including-glazing systems) should be able to be easily removed or modified to allow tenants to express the speciality of their business. Building facades should be constructed of simple, economical materials.

### **Set Backs**

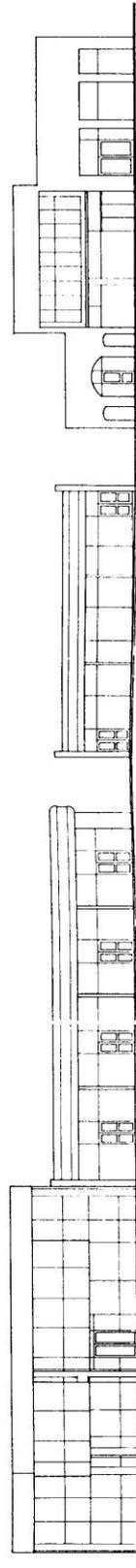
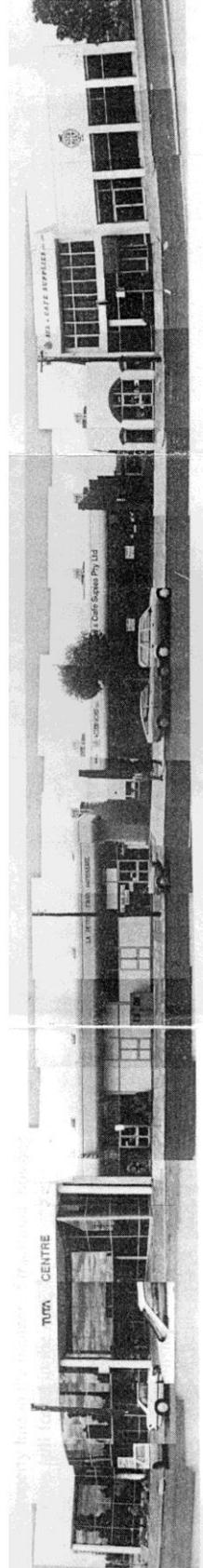
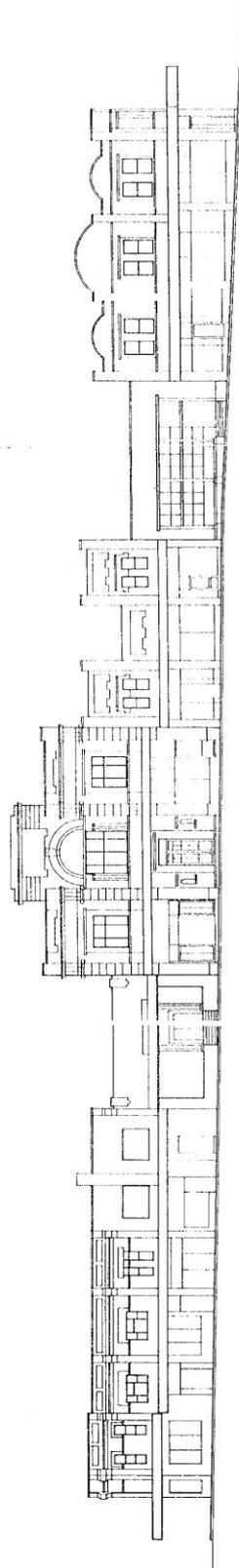
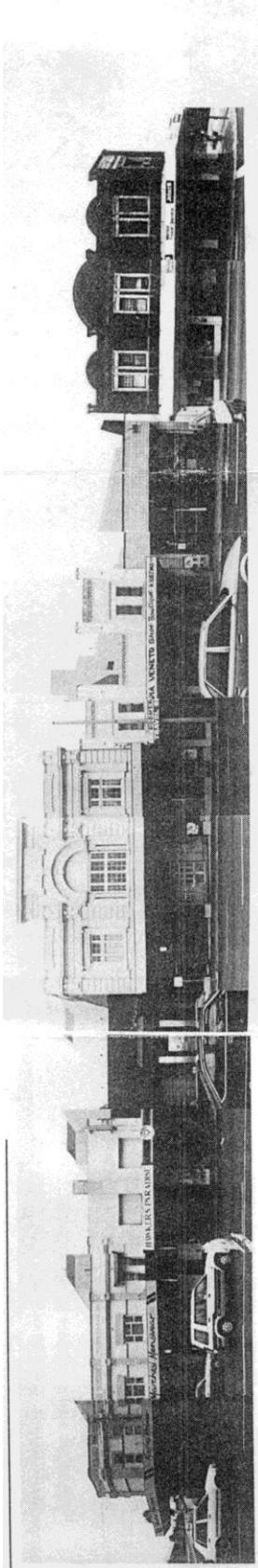
Generally, construct the enclosing walls along the property lines to avoid gaps in the streetscape. Developments that incorporate housing at ground level (restricted to the western sector of Northbridge) could be set back to create a small “semi-public” front yard defined by a fence on the property line in the manner of traditional housing forms still found in the area.

### **Vertical rhythm**

Vertical rhythm, or articulation of adjoining or neighbouring buildings.

### **Horizontal relationships**

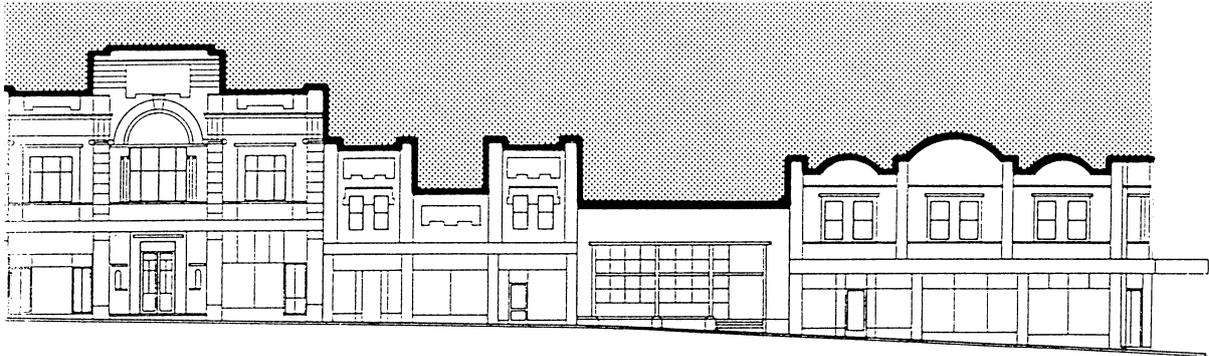
Horizontal relationships with adjoining buildings. Align window heads or sills, glazing, mullions, parapets, balconies, changes in facing materials etc.



### Profile

When infilling an existing streetscape with a new building consider the existing profile created by a group of buildings. New buildings should enhance the streetscape profile.

Street with varied profile.



### Lighting

Incorporate feature lighting to add drama, variety and character to developments at night.

### Interiors

The uniqueness of Northbridge can also be expressed in the interior of buildings, especially the public areas. Design buildings with interesting and unique internal spaces and interior designs.