

Planning Policy Manual – Part 2

**Planning Policies and Design Guidelines for Normalised
Redevelopment Areas**

Section 2.18
East Perth – Area 18
Eastbrook Terrace



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1.0 APPLICATION

The East Brook subdivision is within the Royal Street Central Precinct (Precinct EP3) with a small portion lying in the Boans Precinct (Precinct EP6), as defined in the City of Perth Local Planning Scheme No. 26 (Normalised Redevelopment Areas) (herein called ‘the Scheme’). These Guidelines apply to lots 54, 200 - 207 (inclusive), 217 and 1007 Royal Street and lots 209 - 215 (inclusive) Eastbrook Terrace as shown in Figure 1.

2.0 ABOUT THESE GUIDELINES

The guidelines are divided into two parts. Part 1 applies to all lots and includes a description of the overall character sought for this area, definitions of some terms, and those design elements common to all sites. Part 2 consists of a series of specifications detailing those design considerations that are relevant to particular lots. In any instance where there appears to be a contradiction between the general guidelines and the guidelines for a particular lot, the lot specific guidelines should be followed.

3.0 RELATIONSHIP TO PLANNING SCHEME AND PLANNING POLICIES

These Design Guidelines are intended to supplement the provisions of the Scheme and should be read in conjunction with the Scheme, in particular those provisions relating to the Claisebrook Village Project Area, Precinct EP3: Royal Street Central and Precinct EP6: Boans, and other Planning Policies.

The Deemed Provisions set out in the *Planning and Development (Local Planning Schemes) Regulations 2015* also form part of the Scheme Text.

The Scheme identifies Preferred, Contemplated and Prohibited uses for Precinct EP3: Royal Street Central and Precinct EP6: Boans and stipulates maximum plot ratio. In determining any application for development approval, the local government will have regard to these Design Guidelines, the Scheme and other Planning Policies.

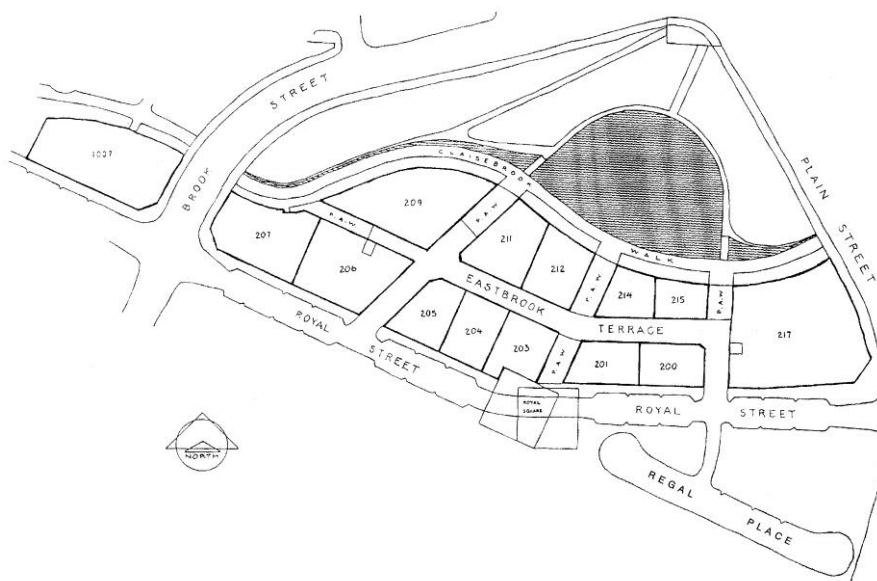


Figure 1:- Eastbrook Subdivision

4.0 PART ONE - GUIDELINES APPLICABLE TO ALL LOTS

4.1 Desired Character

The intent is to create a vibrant mixed use area having the qualities of a traditional urban commercial precinct along Royal Street, with interesting pedestrian access ways linking up with the Claisebrook Walk and parkland along the water's edge. This will be abutted by residential and active uses at ground level and office/commercial and residential development on upper levels. It is anticipated that this area will offer quality office accommodation as an alternative to West Perth and other established commercial areas.

4.2 Preferred Uses

4.2.1 Royal Street

Appropriate uses at street level for Royal Street will have a "shop front" quality providing visual interest for pedestrians, such as retail outlets, small scale showroom activities and customer focussed office activities such as banks and real estate agencies could also be suitable. Offices and residential uses would be suitable for upper floors.

4.2.2 Promenade

Uses at promenade level should be predominantly commercial or active retail with residential or commercial above. In this location active retail uses should be more of an entertainment/cafe flavour, focussed on the corners of the pedestrian access ways and taking advantage of the northern aspect overlooking the water.

4.2.3 Eastbrook Terrace

Uses at Eastbrook Terrace level may be more varied, with the aim being to have some uses with direct pedestrian access from this street. Development should offer visual interest to pedestrians and the more active uses should be clustered at the corners of the north-south pedestrian access ways.

4.2.4 Integration of Art

The involvement of artists in designing a new development can enrich the design response and contribute to the creation of a "sense of place" in the local environment. Consequently the use of artists as part of the design team for new developments is strongly encouraged. Examples of artworks that can be integrated into a new development include detailing to walls, balustrades and railings, paving, shade structures, seating, bins, bollards, drinking fountains, lighting, building fittings, entry treatments and signage.

4.3 Building Envelopes

- The building height and bulk is to be contained within a building envelope, with only minor projections allowed for such items as chimneys, finials, pergolas, small portions of bay and dormer windows, and approved landmark features which in the opinion of the local government contribute to the character and identity of the precinct.
- Balconies, awnings and verandahs may project beyond the building envelope in accordance with the specific guidelines applicable to these features.
- The maximum envelope height is measured from the finished ground level of the highest street or lane level at the perimeter of the lot as shown in Figure 2.

- Note that the building envelope defines the limits of building bulk. It is not intended that the building profile should mirror that of the envelope.

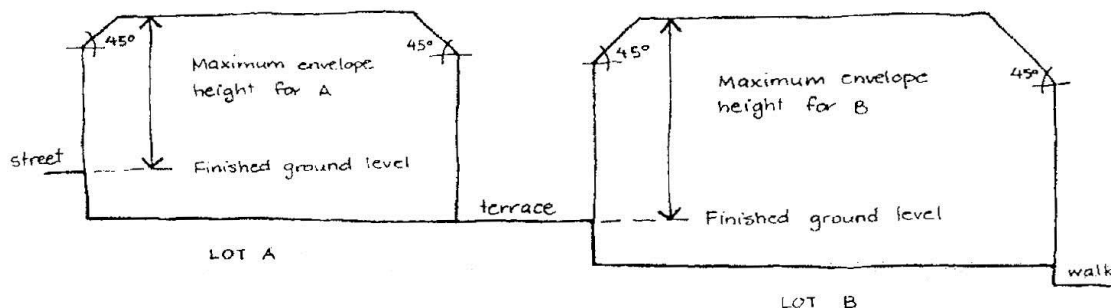


Figure 2:- Measurement of building envelopes

4.4 Balconies, Verandahs and Awnings

- Balconies fronting pedestrian accessways, Eastbrook Terrace, Royal Street, Claisebrook Walk, Plain Street or Brook Street must be incorporated into the building fabric such that they do not project beyond the primary plane of the building elevation and do not project beyond the lot boundary.
- Frontages to Royal Street are required to provide continuous pedestrian protection from sun and rain through the use of a suspended awning or cantilevered verandah 2.5m wide. Posted verandahs will not be permitted along Royal Street.
- Generally a verandah must have a clearance above footpath level of 3.3m. Where necessary verandahs must be stepped to conform with the grade of the footpath. In such cases the steps should not exceed 600mm and the clearance above footpath level may be reduced to a minimum of 3.0m. Figure 3 illustrates this principle.

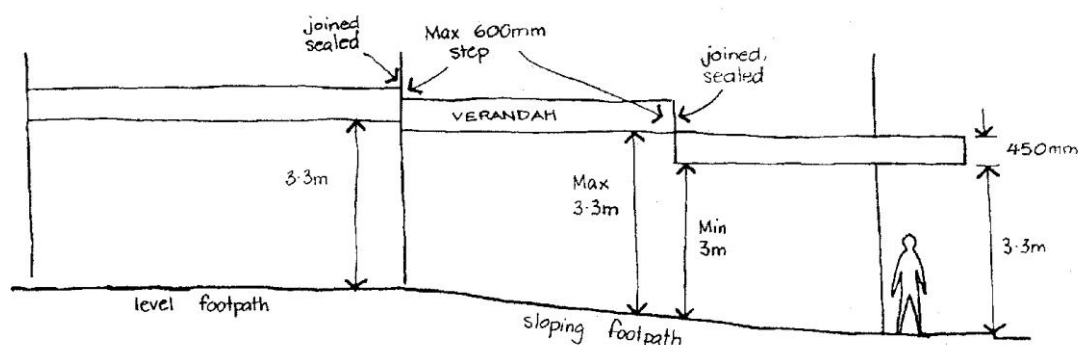


Figure 3:- Clearances and linkages for verandahs and awnings

- Where a verandah abuts another verandah, the two are to be finished so as to prevent rain falling between them. It will be the responsibility of the person erecting the new verandah to effect this result. Figure 3 illustrates this principle. The finished overall depth of the fascia of any verandah is to be 450mm. This is not intended to prevent the inclusion of decorative elements such as pediments, which will be considered on their merits.
- Sun hood type awnings may project up to 1m beyond the lot boundary.
- Any verandah or awning that overhangs or abuts a trafficable street or lane must have 600mm minimum clearance from the likely passage of vehicles, including emergency

vehicles. Allowance should also be made for the unimpeded growth of any nearby street tree.

- In any case the fascia of a verandah or awning must be no less than 600mm closer to the building line than the outer face of the kerb, where one exists. Figure 4 illustrates allowable projections beyond lot boundaries.

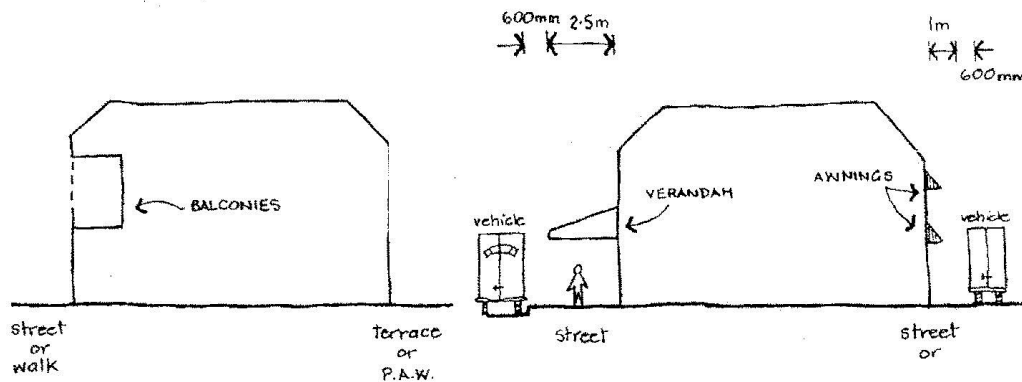


Figure 4:- Allowable projections

4.5 Access and Parking

4.5.1 Pedestrian access

Priority is to be given to maximising pedestrian access and circulation within the subdivision. Pedestrian access off Royal Street is to be at grade, that is, directly from the footpath. Consideration is to be given to designing access suitable for people with impaired mobility wherever possible.

4.5.2 Motor Vehicle and Service Access and Parking

- All parking is to be provided in basements or otherwise adequately concealed from public view to the local government's satisfaction.
- Basement car parks abutting pedestrian access ways, Royal Street, Eastbrook Terrace and Claisebrook Walk should be below ground level as far as possible. Any wall projecting above ground is to be minimal and detailed to provide visual interest and pedestrian scale.
- Ventilation grilles to parking basements are to be screened from Claisebrook Walk. In any case they should be detailed to reduce their visual impact, and be sympathetic in proportion and placement to the building design.
- For designated aged persons' housing a reduced number of parking bays may be negotiated.
- All servicing of buildings is to take place from Eastbrook Terrace or within an internal screened service yard, as appropriate to the nature and scale of the proposed development.
- Provision is to be made for the storage of rubbish bins in such a way as they are screened from public view and can be easily accessed by service vehicles.

4.5.3 Bicycles

Provision is to be made for secure private bicycle parking and suitable end-of-trip facilities.

4.6 Open Space

- Open space is not required for non-residential uses.
- For residential uses an area of private open space directly accessible from a living area is required for each dwelling. This open space may be at ground level or provided as a balcony, terrace or deck.
- All private open space is to be sufficiently dimensioned as to be usable.

4.7 Building Form

4.7.1 Security

Building design should contribute to the creation of a safe environment by avoiding the formation of "blind" spaces which can not be seen from inside the most used rooms. Sites should be well lit and other measures to enhance the personal safety of people within and adjacent to the development should also be considered. Ground floor window design should consider deterrents to vehicle ramming such as raised sill heights.

4.7.2 Roof Form

Generally roofs should be pitched, with a slope of between 30 and 45°. Provision of a decorative parapet with a shallow roof pitch beyond, in the tradition of a shopping street, is acceptable for the Royal Street frontage.

4.7.3 Corners

Corner sites tend to be the most prominent. Buildings situated at the intersections of roads/lanes/pedestrian accessways play a special role in defining the quality of the adjoining public spaces and are often landmarks which assist people's understanding the local environment. Extra height at corners through the use of decorative parapets, tower elements or similar features helps to give prominence to these buildings and is therefore encouraged. In approved instances a "tower element" may take the form of an additional storey if the local government is satisfied that it contributes to the landmark quality of the building.

4.7.4 Articulation

Building forms should be articulated (broken down visually into smaller elements to establish a "vertical rhythm" or pattern, especially at the pedestrian level). At street level this should be at intervals of around 6m - 8m. Projections such as verandahs, canopies, balconies and windows should be used to provide visual interest, an interplay of light and shade, and establish a finer grained scale of development. This will help to achieve visual continuity whilst allowing individual expression for each building.

4.7.5 Detail

The public faces of buildings should be detailed in order to provide visual richness and variety, accentuate vertical rhythms, reduce the visual bulk of building mass and enhance the identity of individual buildings. This can be achieved through the use of colour, texture and materials, surface modelling and the integration of art.

4.7.6 Blank Walls

Blank walls abutting pedestrian access ways, Eastbrook Terrace and Claisebrook Walk are to be kept to a minimum and detailed to provide visual interest and pedestrian scale.

4.7.7 Balconies

The inclusion of balconies to take advantage of views to the water and parkland is encouraged. Balconies can also assist the composition, articulation and visual interest of buildings. They should be of usable dimensions and wherever possible enhanced by the provision of shading from summer sun.

4.8 Materials and Colours

4.8.1 Walls

These should be predominantly limestone, red/terracotta brick or rendered masonry ranging in colour from yellow to red-brown ochre. Sections of corrugated metal decking or timber cladding will be permitted on upper floors.

4.8.2 Windows

The use of reflective or obscure glass is generally not permitted on ground floor windows adjoining pedestrian areas. Rooms requiring privacy should not be situated in such locations. Exterior shade structures should be used where it is necessary to protect windows from summer sun.

4.8.3 Roofs

Roofs should be flat profile tiles in off-white to mid-grey or pale ochre, or corrugated metal decking in Colorbond Off White, Merino, Birch Grey, Wheat or Saltbush. Zinalume finish will only be permitted where it can be demonstrated that glare and reflectivity will not be a problem to neighbours or surrounding activities.

4.9 Retaining Walls and Fences

- If a retaining wall is necessary it is to be no higher than 1.2m high when measured from the adjacent pedestrian zone (e.g. Claisebrook Walk).
- The combined heights of retaining walls and fences is not to exceed 2m when measured from the adjacent pedestrian zone. Where walls extend from retaining walls the materials should be integrated. Walls should be detailed to minimise the sense of bulk and scale.
- Generally, buildings constructed to lot boundaries are preferred over fences. Where infill fencing is required, materials should match the building walls.
- Where fencing abuts Claisebrook Walk, visually permeable materials such as metal railings should be used, although minor solid components may be permitted where residential private open space requires screening.

4.10 Building Services

- All piped and wired services, air conditioners, clothes drying areas, bin stores and water storage tanks are to be concealed from public view. Solar panels and solar water heaters may be visible, provided that they are in the same plane as the roof and there is no alternative location that can provide a similar level of solar efficiency.
- All meters and connections are to be accessed off Eastbrook Terrace, and such fittings should be integrated into the overall landscape and building design.

4.11 Landscape

- In order to increase on-site absorption and reduce run-off, permeable segmental paving is to be used for all hard surfaces.
- Paving to all areas is to be complementary to that in Royal Street and Eastbrook Terrace, with similar size, texture, colour and detailing.
- Landscape themes should be consistent with Claisebrook Cove landscape development generally.

4.12 Crossovers

Vehicle crossovers are to be constructed to the local government's specifications at the developer's expense.

5.0 PART TWO - GUIDELINES APPLICABLE TO SPECIFIC LOTS

5.1 Lot 1007

This important "gateway" site on the north west corner of Brook and Royal Streets is adjacent to the Brook Street pedestrian underpass and is adjoined on two sides by the "Greenway", the public open space network linking East Perth with the city centre. The Greenway is at its narrowest point here, so a building on this site will be required to relate strongly to its landscaped setting and provide an appropriately scaled backdrop for this public area, viewed from within and across the public open space. It will also play a key role in effecting the transition between the closely developed and highly "urban" Royal Street commercial area to the east of Brook Street and the more open form of development to the west which is characterised by very large modern buildings such as the Advanced Manufacturing Technologies Centre, the Education Ministry and Health Department. In contrast, the site is opposite the former East Perth Primary School, a heritage site with small scaled, brick buildings.

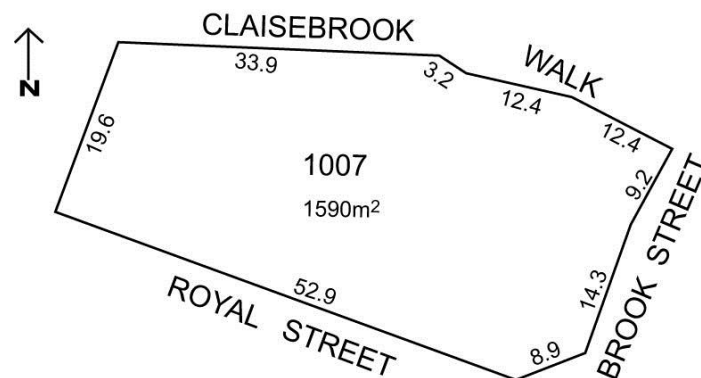


Figure 6: - Lot 1007

5.1.1 Preferred Uses

Commercial uses which will provide visual interest and activity are preferred at street level, with compatible uses such as offices and studios on upper floors. The landscaped setting of the site may lend itself to an alfresco dining establishment or similar on the ground floor facing north or west.

5.1.2 Building Form and Character

- The site is highly visible from all sides, so it will be important that the building presents equally well from every angle, and that service and parking areas are well integrated and

screened. The building design must have cognisance to both the highly urban/commercial eastern end of Royal Street and the more open western end. The building should effect the transition between these zones as far as possible by making a strong corner statement at Brook and Royal Streets to reflect and balance a building on the north east corner of the intersection, then in Royal Street introducing design elements which help tie together the adjoining parkland, the former East Perth School, the Education Ministry and the AMTC buildings.

- The majority area of ground floor facades should be glazed. An interesting and articulated built form is sought. The Brook Street frontage is to be provided with windows and other fenestration - blank walls will not be accepted. Fronting onto Claisebrook Walk and the Greenway parkland, the building could incorporate terraces or other forms of outdoor area for the enjoyment of building occupants and clients.

5.1.3 Building Envelope

The envelope is a maximum of 12m high, projected at 45° from a point 9m above the finished ground level of the Royal Street footpath, and from a point 9m above the finished ground level of Claisebrook Walk along the northern lot perimeter. The building is permitted up to the maximum envelope limit along the remaining lot perimeters. Figure 7 refers.

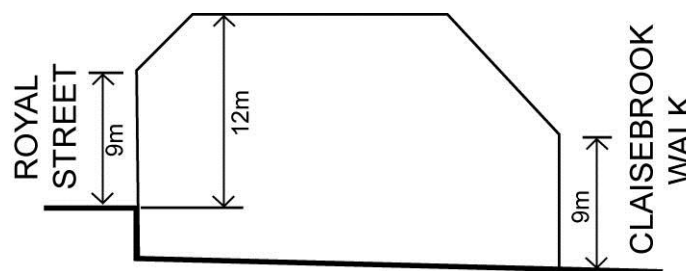


Figure 7:- Building envelope for lot 1007

5.1.4 Building Height

- Overall - minimum 2 storeys (6 metres); maximum 4 storeys (12 metres high).
- Royal Street frontage - predominantly 2 storeys with the possibility of a 3 storey (9 metres) element which may project beyond the building envelope, to emphasise the Royal Street/Brook Street corner.
- Claisebrook Walk frontage - 2 to 3 storeys (6 to 9 metres).
- Brook Street frontage - 2 to 3 storeys (6 to 9 metres) above road level.

5.1.5 Setbacks

- Nil to Royal Street, Claisebrook Walk and public open space, except that a setback from Claisebrook Walk will be considered if the area within the setback is designed to be actively used, such as for alfresco dining or similar.
- Brook Street - It should be noted that there are easements along part of the Brook Street frontage, to which building restrictions apply. A setback of up to 6m from the Brook Street boundary is permitted where it is affected by the easements, but where there is no easement the setback is to be nil.

- Care is to be taken in the treatment of any set back area so as not to create unsafe or litter prone spaces.

5.1.6 Access and Parking

- Pedestrian access is unrestricted. As a minimum, pedestrian access should be available from both the Royal Street and Claisebrook Walk frontages. Building design should emphasise pedestrian entrances. Any gaps between the face of the building and the footpath are to be paved as though they were a continuation of the footpath, using matching materials. Similarly paths or steps connecting with Claisebrook Walk should use matching materials.
- Vehicular access may be gained via a single crossover to Royal Street provided that the crossover is not closer than 20m to the intersection with Brook Street (measured along the Royal Street boundary from the truncation).
- Car Parking: All parking should be screened from view from any public place. The preferred form of car parking is basement parking.

5.1.7 Retaining Walls and Fences

Fencing fronting Claisebrook Walk should be visually permeable, using metal railings or other approved material. Any retaining wall is to be of limestone to match those existing elsewhere in the subdivision.

5.1.8 Easements

Note that this lot is affected by easements associated with the adjacent Brook Street bridge and with stormwater drainage. Title details should be referred to for particulars.

5.2 Lots 200 and 201

These lots front Royal Street and Eastbrook Terrace. Lot 201 abuts the major visual and pedestrian axis within the subdivision, whilst Lot 200 is situated at the intersection of Eastbrook Terrace with Royal Street. Both lots have an important role to play in reinforcing the desired urban streetscape for Royal Street.

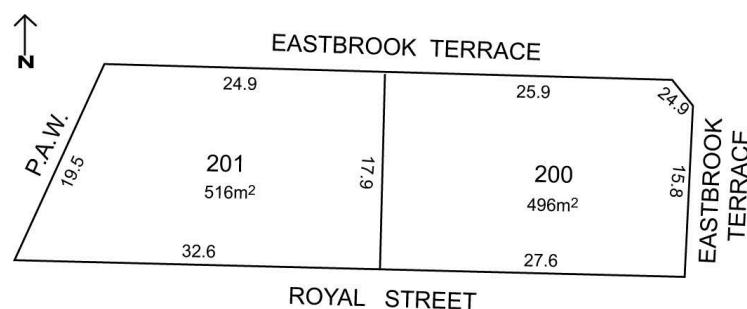


Figure 8:- Lots 200 and 201

5.2.1 Preferred Uses

Active commercial uses which will provide visual interest and activity are preferred at street level, with compatible uses such as offices, studios or housing on upper floors.

5.2.2 Building Form and Character

An interesting and articulated built form with a strong building edge is sought. Along Royal Street the design should reinforce a theme of an urban commercial streetscape with ground floor tenancies displaying traditional "shopfront" articulation, with a vertical emphasis and the majority area of ground floor facades being glazed. Continuous pedestrian weather protection is to be provided along

the Royal Street frontage through the use of verandahs 2.5m wide. Corners should be emphasised to reinforce the "gateway" roles for each of these buildings.

5.2.3 Building Envelope

The envelope is a maximum of 12m high, projected at 45° from a point 9m above the finished ground level of the Royal Street footpath, and from a point 12m above the finished ground level of Eastbrook Terrace along the northern lot perimeter. The building is permitted up to the maximum envelope limit along the remaining lot boundaries.

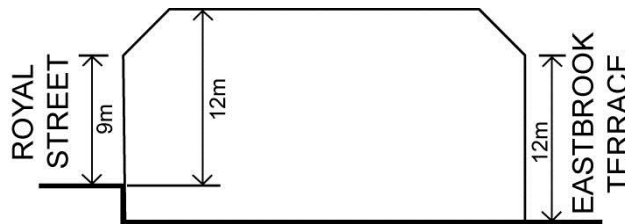


Figure 9:- Building envelope for Lots 200 and 201

5.2.4 Building Height

- Overall - minimum 2 storeys (6 metres); maximum 4 storeys. (12 metres)
- Royal Street frontage – 2 storeys (6 metres). On Lot 201 a 3 storey (9 metres) element on the corner of Royal Street will be considered.
- Eastbrook Terrace and pedestrian accessway frontages – 2 to 4 storeys (6 to 12 metres).

5.2.5 Setbacks

- Nil to all boundaries.

5.2.6 Access and parking

- Pedestrian access is unrestricted. Direct pedestrian access must be available from Royal Street and at least one other frontage. Lot 201 is to have direct pedestrian access from the pedestrian accessway. Building design should emphasise pedestrian entrances.
- Vehicular access may be gained only from Eastbrook Terrace. For Lot 200 the preferred crossover location is 5.0m west along the northern lot boundary. For Lot 201 the preferred crossover location is 9m east along the northern lot boundary.
- Car Parking: Basement parking is preferred. Parking should be screened from view from public spaces.

5.3 Lot 203

This lot occupies a key position at the intersection of Royal Street with the major north-south pedestrian accessway. It fronts "Royal Square", a local visual focus which will incorporate a landmark feature. A building on this lot will play an important role in reinforcing the desired urban streetscape for Royal Street.

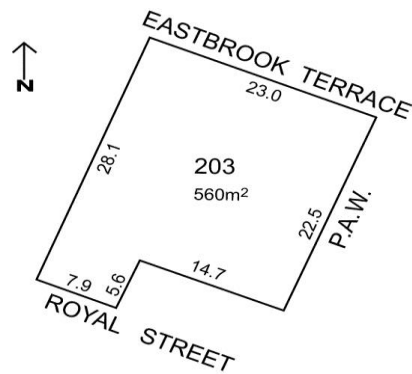


Figure 10:- Lot 203

5.3.1 Preferred Uses

Active commercial uses which will provide visual interest and activity are preferred at street level, with compatible uses such as offices and studios on upper floors.

5.3.2 Building Form and Character

An interesting and articulated built form with a strong building edge is sought. Along Royal Street the design should reinforce a theme of an urban commercial streetscape with ground floor tenancies displaying traditional "shopfront" articulation, with a vertical emphasis and the majority area of ground floor facades being glazed. Continuous pedestrian weather protection is to be provided along the Royal Street frontage through the use of verandahs 2.5m wide. Corners should be emphasised to reinforce the "gateway" role of this building.

5.3.3 Building Envelope

The envelope is a maximum of 12m high, projected at 45° from a point 9m above the finished ground level of the Royal Street footpath, and from a point 12m above the finished ground level of Eastbrook Terrace along the northern lot perimeter. The building is permitted up to the maximum envelope limit along the remaining lot boundaries.

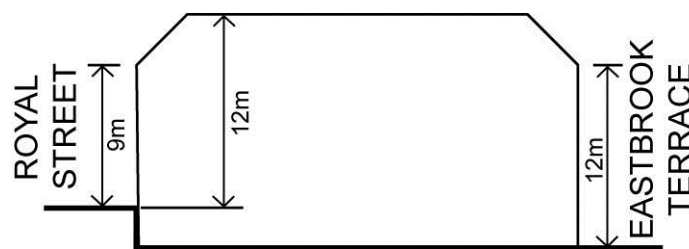


Figure 11:- Building Envelope for Lot 203

5.3.4 Building Height

- Overall - minimum 2 storeys (6 metres); maximum 4 storeys (12 metres).
- Royal Street frontage - 2 storeys (6 metres), with the possibility of a 3 storey (9 metres) element on the corner at Royal Square.
- Eastbrook Terrace and pedestrian accessway frontages - 2 to 4 storeys (6 to 12 metres).

5.3.5 Setbacks

- Nil to all boundaries.

5.3.6 Access and Parking

- Pedestrian access is unrestricted. Direct pedestrian access must be available from Royal Street and the pedestrian accessway frontage. Building design should emphasise pedestrian entrances.
- Vehicular access may be gained only from Eastbrook Terrace. The preferred crossover location is on the northern lot boundary not further than 14m west of the intersection with the pedestrian accessway.
- Car Parking: Basement parking is preferred. Parking should be screened from view from public spaces.

5.4 Lot 204

This lot fronts Royal Street and will play an important role in reinforcing the desired urban streetscape for Royal Street and Eastbrook Terrace.

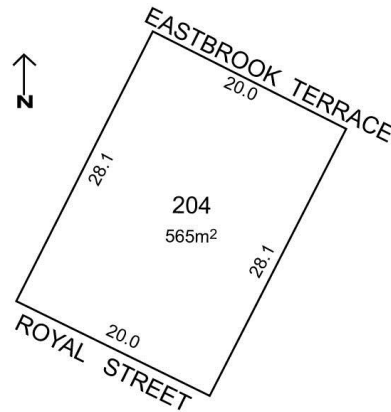


Figure 12:- Lot 204

5.4.1 Preferred Uses

Active commercial uses which will provide visual interest and activity are preferred at street level, with compatible uses such as offices and studios on upper floors.

5.4.2 Building Form and Character

An interesting and articulated built form with a strong building edge is sought. Along Royal Street the design should reinforce a theme of an urban commercial streetscape with ground floor tenancies displaying traditional "shopfront" articulation, with a vertical emphasis and the majority area of ground floor facades being glazed. Continuous pedestrian weather protection is to be provided along the Royal Street frontage through the use of verandahs 2.5m wide.

5.4.3 Building Envelope

The envelope is a maximum of 12m high, projected at 45° from a point 9m above the finished ground level of the Royal Street footpath, and from a point 12m above the finished ground level of Eastbrook Terrace along the northern lot perimeter. The building is permitted up to the maximum envelope limit along the remaining lot boundaries. Figure 13 refers.

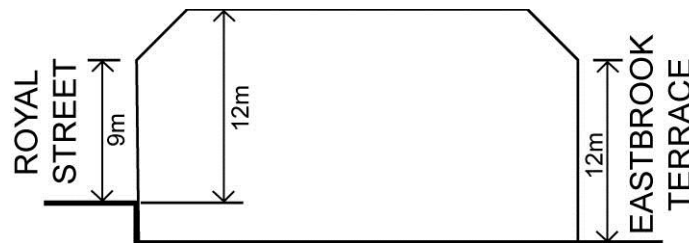


Figure 13:- Building envelope for Lot 204

5.4.4 Building Height

- Overall - minimum 2 storeys (6 metres); maximum 4 storeys. (12 metres)
- Royal Street frontage - 2 storeys (6 metres).
- Eastbrook Terrace and pedestrian accessway frontages - 2 to 4 storeys (6 to 12 metres).

5.4.5 Setbacks

Nil to all boundaries.

5.4.6 Access and Parking

- Pedestrian Access: Direct pedestrian access must be available from both frontages. Building design should emphasise pedestrian entrances.
- Vehicular access may be gained only from Eastbrook Terrace. One crossover is permitted and should be located to avoid the existing street tree. As an alternative, access from Eastbrook Terrace could be shared with Lot 203 and/or Lot 205, subject to satisfactory arrangements being made between the owners to ensure that a right of carriageway is preserved for the life of the building on this lot.
- Car Parking: Basement parking is preferred. Parking should be screened from view from public spaces.

5.5 Lot 205

This lot is important for reinforcing the desired urban streetscape for Royal Street as well as occupying a key position at the entrance to Eastbrook Terrace.

5.5.1 Preferred Uses

Active commercial uses which will provide visual interest and activity are preferred at street level, with compatible uses such as offices and studios on upper floors.

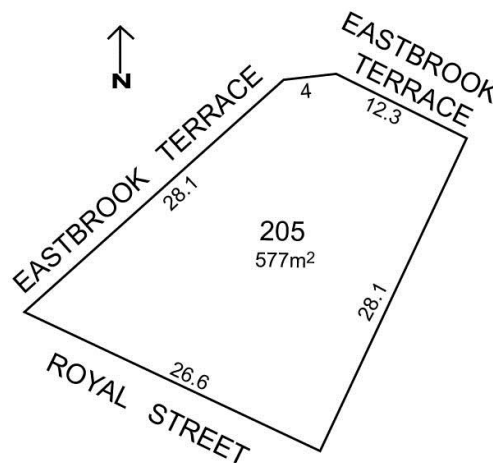


Figure 14:- Lot 205

5.5.2 Building Form and Character

An interesting and articulated built form with a strong building edge is sought. Along Royal Street the design should reinforce a theme of an urban commercial streetscape with ground floor tenancies displaying traditional "shopfront" articulation, with a vertical emphasis and the majority area of ground floor facades being glazed. Continuous pedestrian weather protection is to be provided along the Royal Street frontage through the use of verandahs 2.5m wide. Corners should be emphasised to reinforce the "gateway" role this building will perform at the intersection of Royal Street and Eastbrook Terrace. The building should strongly address all public site edges, with particular emphasis given to the treatment of the Royal Street/Eastbrook Terrace corner.

5.5.3 Building Envelope

The envelope is a maximum of 12m high, projected at 45° from a point 9m above the finished ground level of the Royal Street footpath, and from a point 12m above the finished ground level of Eastbrook Terrace along the northern lot perimeter. The building is permitted up to the maximum envelope limit along the remaining lot perimeters. Figure 15 refers.

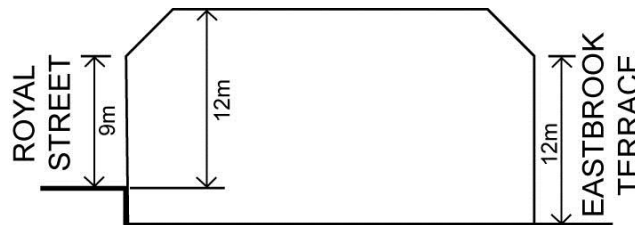


Figure 15:- Building Envelope for Lot 205

5.5.4 Building Height

- Royal Street frontage - 2 storeys (6 metres).
- Eastbrook Terrace frontages - 2 - 4 storeys (6 to 12 metres).

5.5.5 Setbacks

- Nil to all boundaries.

5.5.6 Access and Parking

- Pedestrian access is unrestricted. Direct pedestrian access must be available from Royal Street and the western frontage to Eastbrook Terrace. Building design should emphasise pedestrian entrances.
- Vehicular access may be gained only from Eastbrook Terrace. The preferred crossover location is on the northern lot boundary. The crossover should be a safe distance from the bend in Eastbrook Terrace whilst avoiding the necessity to move the existing street tree if possible.
- Car Parking: Basement parking is preferred. Parking should be screened from view from public spaces.

5.6 Lot 206

This site is situated at the entrance to Eastbrook Terrace from Royal Street and accordingly will perform an important "gateway" role by introducing both pedestrian and vehicular traffic into the subdivision, and leading the eye towards the water and Greenway. It will be important in reinforcing the desired urban streetscape in Royal Street.

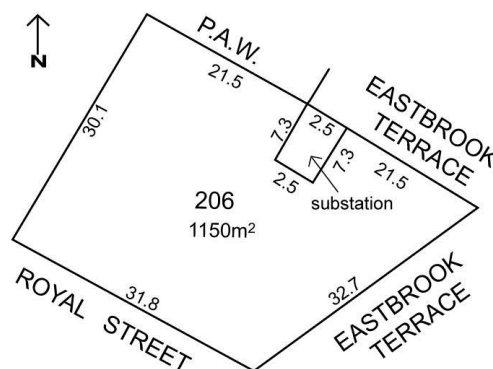


Figure 16:- Lot 206

5.6.1 Preferred Uses

Active commercial uses which will provide visual interest and activity are preferred at street level, with compatible uses such as offices, studios or housing on upper floors.

5.6.2 Building Form and Character

The building will perform a "gateway" function at the intersection of Royal Street and Eastbrook Terrace and this should be reflected in the design. It should strongly address all public site edges, with particular emphasis given to the treatment of the Royal Street/Eastbrook Terrace corner. An interesting and articulated built form with a vertical emphasis is sought. Along Royal Street the design should reinforce a theme of an urban commercial streetscape with ground floor tenancies displaying traditional "shopfront" articulation, with a vertical emphasis and the majority area of ground floor facades being glazed. Continuous pedestrian weather protection is to be provided along the Royal Street frontage through the use of verandahs 2.5m wide. Corners should be emphasised to reinforce the "gateway" role of this building.

5.6.3 Building Envelope

The envelope is a maximum of 12m high, projected at 45° from a point 9m above the finished ground level of the Royal Street footpath, and from a point 12m above the finished ground level of Eastbrook Terrace along the northern lot perimeter. The building is permitted up to the maximum envelope limit along the remaining lot perimeters. Figure 17 refers.

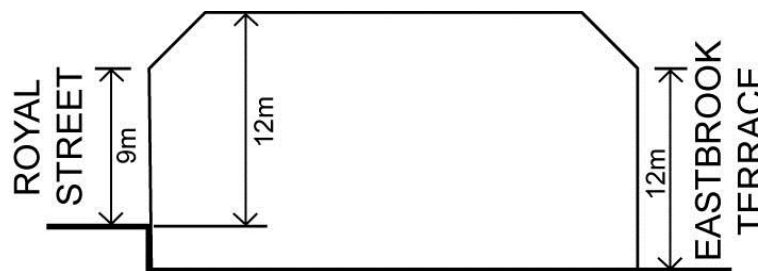


Figure 17:- Building Envelope for Lot 206

5.6.4 Building Height

- Overall - Minimum 2 storeys (6 metres); maximum 4 storeys (12 metres).
- Royal Street frontage - 2 storeys (6 metres).
- Eastbrook Terrace frontages - 2 - 4 storeys (6 to 12 metres).

5.6.5 Setbacks

- Nil to all boundaries.

5.6.6 Access and Parking

- Pedestrian access is unrestricted. As a minimum, pedestrian access should be available from Royal Street and the eastern boundary with Eastbrook Terrace. Building design should emphasise pedestrian entrances.
- Vehicular access maybe gained via a single crossover on the northern site boundary not closer than 10m to the easternmost corner of the site.
- Car Parking: The preferred form of car parking for this lot is basement parking. All parking should be screened from public view.

5.6.7 Substation

The electricity sub-station on this site is to be incorporated into the new building in such a way as to minimise its visibility from public spaces. To this end the sub-station has been designed so that its roof can be used as a floor slab and the parapet of its outer skin can be removed and a suspended slab supported. Construction drawings of the substation will be made available for design purposes.

5.7 Lot 207

This is a prominent corner site at the western entrance to the Triangle sector subdivision, adjacent to and slightly elevated above the Brook Street pedestrian underpass and promenade which forms part of the Greenway linking East Perth with the city centre. A building on this site will play a key role in introducing the desired qualities of an urban streetscape for Royal Street.

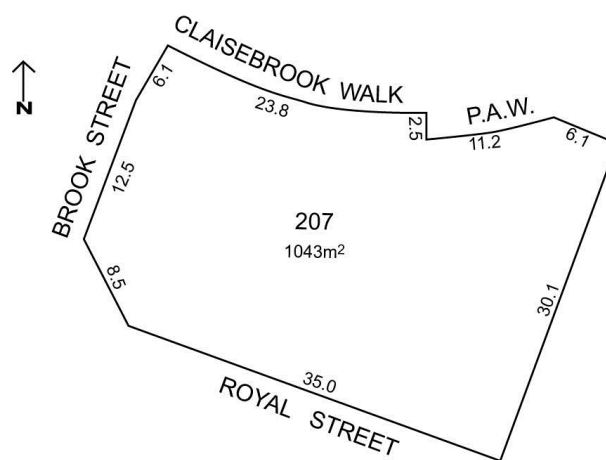


Figure 18:- Lot 207

5.7.1 Preferred Uses

Active commercial uses which will provide visual interest and activity are preferred at street level, with compatible uses such as offices, studios or housing on upper floors.

5.7.2 Building Form and Character

Because of the highly visible "gateway" location of this site it is important that the building strongly addresses all public site edges, with particular emphasis given to treatment of the Royal Street/Brook Street corner. An interesting and articulated built form with a strong building edge is sought. Along Royal Street the design should reinforce a theme of an urban commercial streetscape with ground floor tenancies displaying traditional "shopfront" articulation, with a vertical emphasis and the majority area of ground floor facades being glazed. Continuous pedestrian weather protection is to be provided along the Royal Street frontage through the use of verandahs 2.5m wide. Corners should be emphasised to reinforce the "gateway" roles for each of these buildings. The Brook Street frontage is to be provided with windows and other fenestration - blank walls will not be accepted. If appropriate and necessary the removal of any bridge railings abutting the building can be negotiated.

5.7.3 Building Envelope

The envelope is a maximum of 12m high, projected at 45° from a point 9m above the finished ground level of the Royal Street footpath, and from a point 12m above the finished ground level of Eastbrook Terrace along the northern lot perimeter. The building is permitted up to the maximum envelope limit along the remaining lot perimeters. Figure 19 refers.

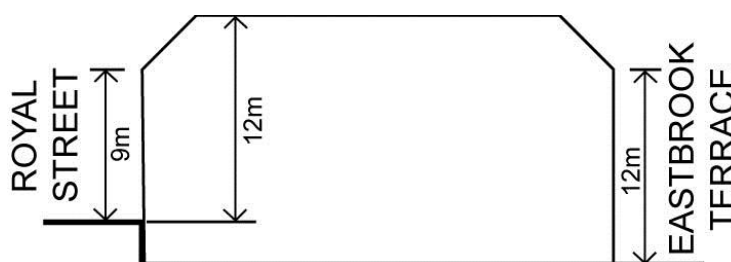


Figure 19:- Building Envelope for Lot 207

5.7.4 Building Height

- Over all - minimum 2 storeys (6 metres); maximum 4 storeys (12 metres).
- Royal Street frontage - predominantly 2 storeys (6 metres) with the possibility of a 3 storey (9 metres) element which may project beyond the building envelope, to emphasise the Royal Street/Brook Street corner.
- Eastbrook Terrace and pedestrian accessway frontages - 2 to 4 storeys (6 to 12 metres). Claisebrook Walk frontage - 3 to 4 storeys (9 to 12 metres).
- Brook Street frontage - 2 to 3 storeys (6 to 9 metres) above road level.

5.7.5 Setbacks

Nil to all boundaries. It should be noted that there is an easement on this lot along the Brook Street frontage, to which building restrictions apply. Notwithstanding these restrictions, it is expected that there will be no uncovered gap between the building and the footpath.

5.7.6 Access and Parking

- Pedestrian access is unrestricted. As a minimum, pedestrian access should be available from both the Royal Street and Eastbrook Terrace frontages. Building design should emphasise pedestrian entrances.
- Vehicular access may be gained via a single crossover to Royal Street provided that the crossover is not closer than 20m to the intersection with Brook Street (measured along the Royal Street boundary from the truncation). As an alternative, access from Eastbrook Terrace could be shared with Lot 206, subject to satisfactory arrangements being made between the owners to ensure that a right of carriageway is preserved for the life of the building.
- Car Parking: All parking should be screened from view from any public place. The preferred form of car parking is basement parking.

5.7.7 Retaining Walls and Fences

Fencing fronting Claisebrook Walk should be visually permeable, using metal railings or other approved material. As this site is already elevated above Claisebrook Walk by a retaining wall the use of solid fencing would result in an unacceptably high wall adjacent to a pedestrian area.

5.7.8 Easement

Note that this lot is affected by an easement associated with the adjacent Brook Street bridge. Title details should be referred to for particulars.

5.8 Lot 209

This is a large sloping site facing Brook Street. It is situated at the junction of the pedestrian accessway leading from Royal Street with the pedestrian promenade and overlooks the lake and parkland. Its long outwardly curving boundary gives it excellent northern exposure and will also make a building on this site probably the most prominent along this section of Claisebrook Walk.

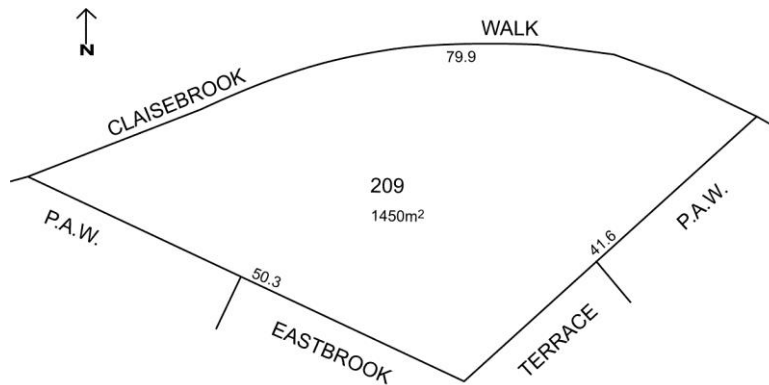


Figure 20: - Lot 209

5.8.1 Preferred Use

The preferred uses for this site are residential and commercial, with entertainment focussed retail such as a cafe a possibility at ground level adjacent to the pedestrian access and facing Claisebrook Walk.

5.8.2 Building Form and Character

- The northern elevation of this building is of particular importance as it will occupy a long frontage visible from Brook Street. For any residential usage, as many units as possible should be given living rooms and private open space with a northerly aspect.
- Along Claisebrook Walk frontage this building should be designed to present a very strong and well detailed "wall" of development. Balconies should present as "outdoor rooms" within the primary plane of the building rather than as projections along this frontage.
- Balconies and bay windows along the Eastbrook Terrace and pedestrian access frontages are encouraged in order to enhance the intimate, enclosed feel and natural surveillance of these places.

5.8.3 Building Envelope

The building envelope is a maximum of 15m high, projected at 45° from a point 12m above the finished ground level of Eastbrook Terrace along the southern lot boundary, and from a point 15m above the finished ground level of Claisebrook Walk at the northern lot boundary. The building is permitted up to the maximum envelope limit along the remaining lot boundary. Figure 21 refers.

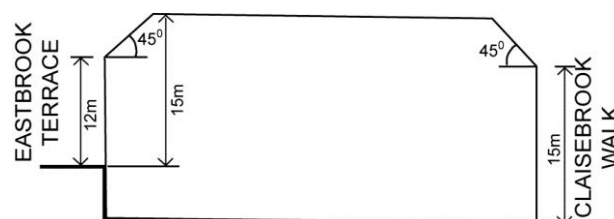


Figure 21:- Building Envelope for lot 209

5.8.4 Building Height

- Overall - minimum 2 storeys; maximum 5 storeys (6 to 15 metres).
- Eastbrook Terrace frontage - 2 to 3 storeys (6 to 9 metres).
- Claisebrook Walk frontage - 3 to 5 storeys (9 to 15 metres).

5.8.5 Setbacks

- Nil to all boundaries.

5.8.6 Access and Parking

- Pedestrian access is unrestricted. Pedestrian access to residences should be individual for any ground floor units, where possible, or shared between a small number of dwellings. A major pedestrian entry should be situated on the south-eastern corner of the building. Pedestrian access for residential and non-residential uses should be separate. Occupants must be able to access the basement car parking from within the building.
- Vehicle access is restricted to Eastbrook Terrace. The preferred crossover location is on the eastern lot boundary not closer than 5m from the south-east corner of the lot.
- Car Parking: The preferred form of parking for this site is basement parking. All parking should be screened from public view.
- Service Access: Consideration must be given to service access and the provision of bin stores. The impact of services on the ground floor environment must be minimised.

5.8.7 Retaining Walls and Fences

Fencing fronting Claisebrook Walk should be visually permeable, using metal railings or other approved material. Any retaining wall should be of limestone to match those existing elsewhere within the subdivision.

5.9 Lots 211 & 212

These lots are centrally located between the two major pedestrian accessways which lead from Royal Street to the public promenade along the water's edge. The lots slope down from Eastbrook Terrace towards Claisebrook Walk and enjoy views across the water and parkland. These lots will be highly visible from both Plain Street and Brook Street.



Figure 22:- Lots 211 and 212

5.9.1 Preferred Uses

The preferred uses for these sites are residential and commercial. There is potential for entertainment focussed retail such as cafe to be located adjacent to Claisebrook Walk and pedestrian accessways. Other active commercial uses will be considered on the ground floor.

5.9.2 Building Form and Character

- Being highly visible from beyond the subdivision, it is important that the northern elevations of these buildings are designed to present a very strong and well detailed "wall" of development. Balconies should present as "outdoor rooms" within the primary plane of the buildings rather than as projections along this frontage. All elevations should have regard to the pedestrian environment and be detailed and scaled accordingly. Overall the buildings should have a vertical emphasis.
- Balconies and bay windows along the Eastbrook Terrace and pedestrian accessway frontages are encouraged in order to enhance the intimate, enclosed feel and natural surveillance of these places.
- In the case of residential units, as many dwellings as possible should be given living rooms and private open space with a northerly aspect. As far as practical, dwellings should be given a sense of "address" through the use of individual entrances or other devices.

5.9.3 Building Envelope

The envelope is a maximum of 15m high, projected at 45 degrees from a point 12m above the finished ground level of Eastbrook Terrace along the southern lot boundary, and from a point 15m above the finished ground level of Claisebrook Walk at the northern lot boundary. The building is permitted up to the maximum envelope limit along the remaining lot boundaries. Figure 23 refers.

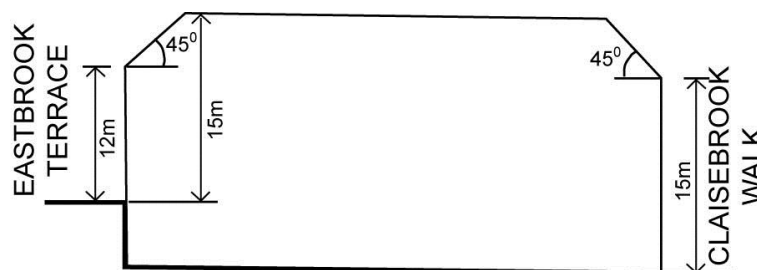


Figure 23:- Lots 211 and 212

5.9.4 Building Height

- Overall - 2 to 5 storeys (6 to 15metres).
- Eastbrook Terrace frontage - 2 to 4 storeys (6 to 12 metres).
- Claisebrook Walk frontage - 3 to 5 storeys (9 to 15 metres), with particular emphasis given on lot 212 to the intersection of Claisebrook Walk and the pedestrian accessway.

5.9.5 Setbacks

- Nil to all boundaries.

5.9.6 Access and Parking

- Pedestrian access is unrestricted but as a minimum must be provided from the pedestrian accessway and Eastbrook Terrace. Emphasis should be given to pedestrian entrances to the building, with separate entrances for residential and non-residential activities.
- Vehicle access is limited to Eastbrook Terrace.
- Car Parking: Basement parking is preferred, if possible, but direct access to parking from Eastbrook Terrace will be considered if it is adequately screened from public view. Parking via a single crossover shared between both lots will be considered.
- Service Access: consideration must be given to service access and the provision of bin stores for the development. The impact of services on the ground floor environment must be minimised. Access for service vehicles is limited to the east-west running section of Eastbrook Terrace for Lot 211.

5.9.7 Retaining Walls and Fences

Fencing fronting Claisebrook Walk should be visually permeable, using metal railings or other approved material. Any retaining wall should be of limestone to match those existing elsewhere within the subdivision.

5.10 Lots 214 and 215

These lots overlook the pond and parkland. They terminate the vista from the Plain Street roundabout. Lot 214 is adjacent to the major visual axis through the subdivision.

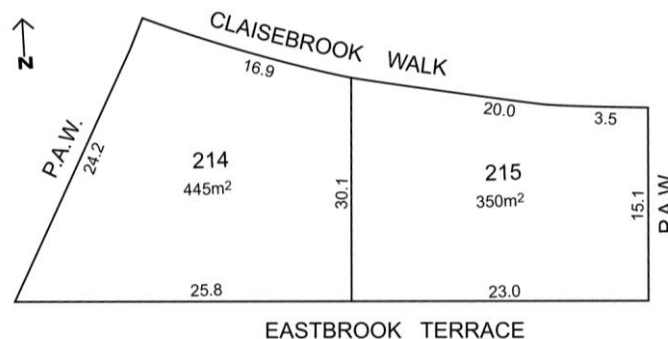


Figure 24:- Lots 214 and 215

5.10.1 Preferred Uses

The preferred use for these lots is commercial at ground floor levels with commercial uses or housing above. There is potential for entertainment focussed retail such as a cafe to be located adjacent to Claisebrook Walk and pedestrian accessways.

5.10.2 Building Form and Character

- Being highly visible from beyond the subdivision, it is important that the northern elevations of these buildings are designed to present a very strong and well detailed "wall" of development. Balconies should present as "outdoor rooms" within the primary plane of the buildings rather than as projections along this frontage. All elevations should have regard to the pedestrian environment and be detailed and scaled accordingly. Overall the buildings should have a vertical emphasis.
- Particular emphasis should be given on Lot 214 to the intersection of Claisebrook Walk with the adjacent pedestrian accessway, which is the major pedestrian and visual axis within the subdivision.

- Balconies and bay windows along the Eastbrook Terrace and pedestrian accessway frontages are encouraged in order to enhance the intimate, enclosed feel and natural surveillance of these places.
- Any dwellings should be given living rooms and private open space with a northerly aspect if possible.

5.10.3 Building Envelope

The envelope is a maximum of 15m high, projected at 45° from a point 12m above the finished ground level of Eastbrook Terrace along the southern lot boundary, and from a point 15m above the finished ground level of the public promenade at the northern lot boundary. The building is permitted up to the maximum envelope limit along the remaining lot boundaries. Figure 25 refers.

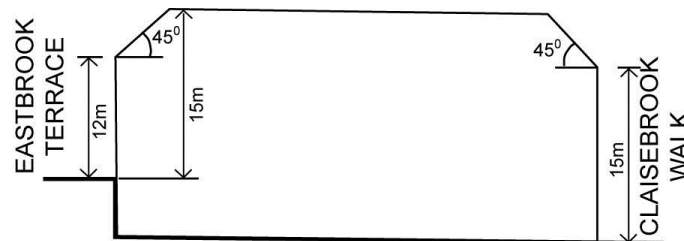


Figure 25:- Building envelope for Lots 214 and 215

5.10.4 Building Height

- Overall - 2 -5 storeys (6 to 15 metres).
- Eastbrook Terrace frontage - 2 - 4 storeys (6 to 12 metres).
- Promenade frontage - 3 - 5 storeys (9 to 15 metres).

5.10.5 Setbacks

- Nil to all boundaries.

5.10.6 Access and Parking

- Pedestrian access is unrestricted but as a minimum must be provided from the pedestrian accessway and Eastbrook Terrace. Emphasis should be given to pedestrian entrances to the building, with separate entrances for residential and non-residential activities.
- Vehicle access is limited to Eastbrook Terrace.
- Car Parking: Basement parking is preferred, if possible, but direct access to parking from Eastbrook Terrace will be considered if it is adequately screened from public view. Parking via a single crossover shared between both lots will be considered.

5.10.7 Retaining Walls and Fences

Fencing fronting Claisebrook Walk should be visually permeable, using metal railings or other approved material. Any retaining wall should be of limestone to match those existing elsewhere within the subdivision.

5.11 Lot 217

This is a large and highly visible corner site situated at the eastern end of the Eastbrook subdivision. It has views across the lake and parkland, north and south along Plain Street and across Claisebrook

Cove. It is adjacent to the Plain Street pedestrian underpass and Claisebrook Walk, which form part of the Greenway linking East Perth with the city centre.

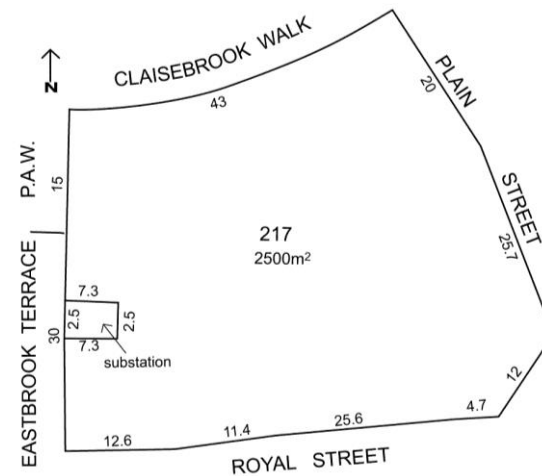


Figure 26:- Lot 217

5.11.1 Preferred Uses

Being a very prominent site with a significant northern aspect, a wide range of uses could be suitable for this site. At ground level facing Royal Street, preference will be given to active commercial uses which will provide visual interest and activity at street level. The site may also be suitable for serviced apartments, major corporate offices and residential activities.

5.11.2 Building Form and Character

- The building should have a strong vertical emphasis to highlight its gateway role and to take advantage of its high exposure. The bulk of the building should be located against the Royal Street and Plain Street frontages. The opportunity exists for the building to incorporate a central courtyard.
- It is important that the building strongly addresses all public site edges, with particular emphasis given to treatment of the Royal Street/Plain Street corner. Along Royal Street the design should reinforce a theme of an urban commercial streetscape with ground floor tenancies displaying traditional "shopfront" articulation, with a vertical emphasis and the majority area of ground floor facades being glazed. Continuous pedestrian weather protection is to be provided along the Royal Street frontage through the use of verandahs 2.5m wide.
- Being highly visible from beyond the subdivision, it is important that the northern elevations of this building is designed to present a very strong and well detailed "wall" of development. Balconies should present as "outdoor rooms" within the primary plane of the buildings rather than as projections along this frontage. All elevations should have regard to the pedestrian environment and be detailed and scaled accordingly.
- Balconies and bay windows along the Eastbrook Terrace and pedestrian accessway frontages are encouraged in order to enhance the intimate, enclosed feel and natural surveillance of these places.
- The Plain Street frontage is to be provided with windows and other fenestration - blank walls will not be accepted. If appropriate and necessary the removal of any bridge railings abutting the building can be negotiated.

5.11.3 Building Envelope

The envelope is a maximum of 15m high, projected at 45 degrees from a point 12m above the finished ground level of Royal Street along the southern lot boundary, and from a point 15m above the finished ground level Claisebrook Walk at the northern lot boundary. The building is permitted up to the maximum envelope limit along the remaining lot boundaries. Figure 27 refers.

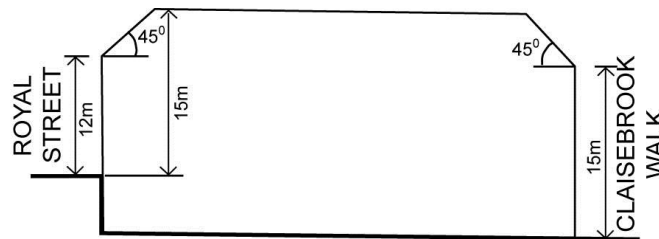


Figure 27:- Building envelope Lot 217

5.11.4 Building Height

- Overall - minimum 2 storeys (6 metres); maximum 5 storeys (15 metres).
- Royal Street frontage - predominantly 2 storeys (6 metres).
- Eastbrook Terrace frontage - 2 to 4 storeys. (6 to 12 metres)
- Claisebrook Walk frontage - 3 to 5 storeys (9 to 15 metres).
- Plain Street frontage - 2 to 3 storeys above road level (6 to 9 metres).

5.11.5 Setbacks

- Nil to all boundaries.

5.11.6 Access and Parking

- Pedestrian access is unrestricted but at a minimum must be provided from the Royal Street and western (Eastbrook Terrace) frontages, with particular emphasis being given to an entrance on the corner of Royal Street and Plain Street.
- Vehicular access for parking and servicing is to be via a crossover from Eastbrook Terrace on the north side of the electricity sub-station.
- Car Parking must be screened from view of any public place. The preferred form of parking for this site is basement parking.

5.11.7 Substation

The electricity sub-station on this site is to be incorporated into the new building in such a way as to minimise its visibility from public spaces. To this end the sub-station has been designed so that its roof can be used as a floor slab and the parapet of its outer skin can be removed and a suspended slab supported. Construction drawings of the substation will be made available for design purposes.

5.11.8 Retaining Walls and Fences

Fencing fronting the promenade should be visually permeable, using metal railings or other approved material. Any retaining wall is to be of limestone to match those existing elsewhere in the subdivision.

5.11.9 Easement

Note that this lot is affected by an easement relating to the adjacent Plain Street bridge. Title details should be referred to for particulars.