



City of Perth
City Planning Scheme
No. **2**

Planning Policy Manual – Part 2

Planning Policies and Design Guidelines for Normalised Redevelopment Areas

Section 2.31

East Perth – Area 31

Brown and Kensington Street West

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1.0 APPLICATION

The land bounded by Claisebrook Road, Kensington Street, Brown Street and the rear of lots fronting East Parade is within the Boans Precinct (Precinct EP6), as defined in the City of Perth Local Planning Scheme No. 26 (Normalised Redevelopment Areas) (herein called ‘the Scheme’). These Guidelines apply to that area as shown in Figure 1.

2.0 CONTEXT

Precinct EP6 is known as the ‘Boans’ Precinct and contains a mix of older style East Perth commercial and residential uses. West of East Parade, the precinct has a finer and more ‘intense’ urban grain reflecting the established warehouse and commercial nature of uses in the area. The “New Saunders Street” (former Saunders Street warehouse site) is currently being redeveloped with guidelines in place seeking that new development is complimentary with the Boans site adjacent. This former Saunders Street warehouse site is a Heritage Council State Registered place of significance. Westrail land within the rail reserve is earmarked to become an overflow public car park once the land is no longer required by Westrail.



Figure 1:- Brown Street.

2.1 General Site Characteristics and Requirements

The site is characterised by a reasonably consistent urban form consisting of buildings one to two storeys in height. Variation mainly exists through setback distances and building types which reflect different uses and periods of development. Buildings fronting Kensington Street are mainly at a nil setback with a number of sites which are open to the street.

Buildings along Brown Street are generally setback between three metres and approximately ten metres. The larger setbacks usually contain vehicle standing/parking areas.

The precinct offers a consistency of scale and use type, being small scale service industry commercial and light industrial. The buildings are representative of two primary ‘planning control’ periods - before and after parking provision was an issue. Older buildings are without setback and address the street. Some of these have vehicle crossovers to accommodate vehicle access, vehicle servicing (this being a use of more than one premises in the area) and service deliveries. The newer buildings have larger setback areas which generally accommodate parking to some degree. Due to the size of the lots and their frontage widths, parking in these areas tends to be inefficient, without there being sufficient depth or width for vehicle movement and parking.

Building height is generally one to two storeys. Floor to floor heights are generally high given the commercial and working nature of uses.

Some representation of small scale warehouse architecture remains, with "saw-toothed" roofs, red brickwork (limited) etc. These are felt to represent positive industrial aesthetic qualities of the area.

The street pattern in the area is remnant of the former grid structure and is clear and legible. Future proposals include the extension of Kensington Street to the west across the northern end of the AMTC. This alignment currently ends at the pedestrian footbridge to Claisebrook Station, which closes the vista in that direction.

Streets have been upgraded - at least in part - with Plane Trees maturing and having a notable impact on the streetscape.

In general, a consistent built up frontage will be sought for all streets in the precinct. The existing built form and 'grain' will be reinforced through the application of the guidelines.

The height of buildings in the precinct will be limited, two to four storeys (6m to 14m max to apex of roof) in accordance with building envelopes.

Materials in new development will be encouraged to incorporate a component of red brickwork consistent with that of heritage buildings in East Perth. Traditional iron or steel roofing materials will be encouraged. Other materials should reflect a modern interpretation of the warehouse and light industrial building aesthetic such as rendered masonry with the potential to incorporate sections of profiled steel panels, steel framing and glazing sections.

Buildings should primarily be of a solid construction nature such as brickwork, stone masonry or rendered concrete type materials, with lighter materials used for suspended, lightweight or cantilevered building elements such as awnings, feature panels, roof sections and balconies.

2.2 Scheme Provisions

These Design Guidelines are intended to supplement the provisions of the Scheme and should be read in conjunction with the Scheme, in particular those provisions relating to the Claisebrook Village Project Area, Precinct EP6: Boans, and other Planning Policies.

The Deemed Provisions set out in the *Planning and Development (Local Planning Schemes) Regulations 2015* also form part of the Scheme Text.

The Scheme identifies Preferred, Contemplated and Prohibited uses for Precinct EP6: Boans and Stipulates maximum plot ratio. In determining any application for development approval, the local government will have regard to these Design Guidelines, the Scheme and other Planning Policies.

This Precinct is intended to house a mix of moderately scaled enterprises and residential development. It is anticipated that Brown Street will become predominantly residential in nature, where Kensington Street should retain the commercial and enterprise uses which currently exist.

New development should be in harmony with the character and scale established by existing buildings, especially in the vicinity of those of heritage value. The local government encourages the recycling of buildings where this is viable and where they make a positive contribution to the streetscape and amenity of the area. A number of buildings in the precinct are felt to contribute to the character of the precinct. The retention and re-use of these buildings would be encouraged.

3.0 BUILDING FORM

3.1 Setbacks

SETBACKS FRONTAGE	MIN	MAX
Claisebrook Road	Nil	-
Brown Street	3m	-
Kensington Street	Nil	-
Side and Rear Setbacks	Nil*	-

*Nil- other than corner lots which are to be as otherwise noted

Other than those stated above, setbacks for walls with openings for residential developments are to be in accordance with the R-Codes. Balconies fronting boundaries with a nil setback must be incorporated into the building fabric such that they do not project beyond the primary plane of the building elevation.

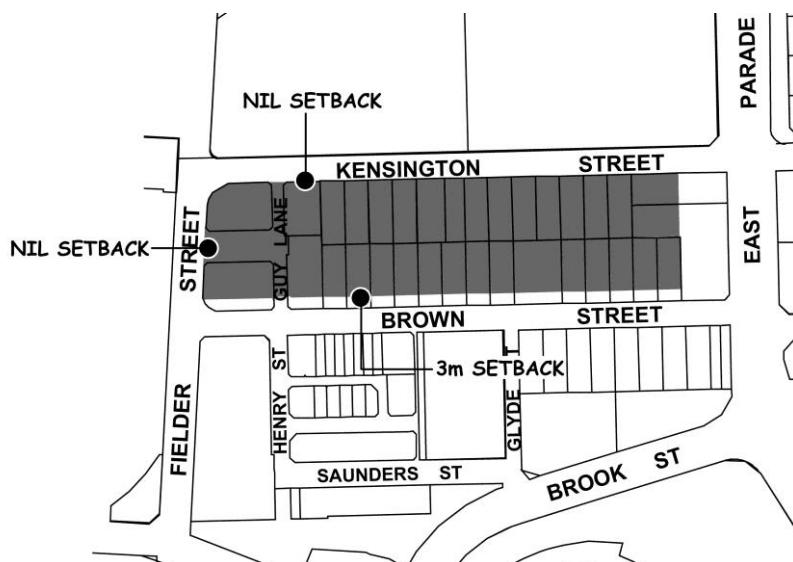


Figure 2:- Setbacks.

3.2 Building Height

Building height and bulk will be required to be contained within building envelopes. Only minor projections will be allowed through building envelopes for such items as chimneys, awnings, pergolas, balconies and small portions of bay and dormer windows.

Buildings facing Kensington Street can be up to four storeys in height (14m max to apex of roof) reducing in height towards the rear (southern) boundary.

Buildings facing Brown Street can be up to three storeys (9m) at the setback line with building envelopes increasing in height to a maximum of four storeys (14m max to apex of roof).

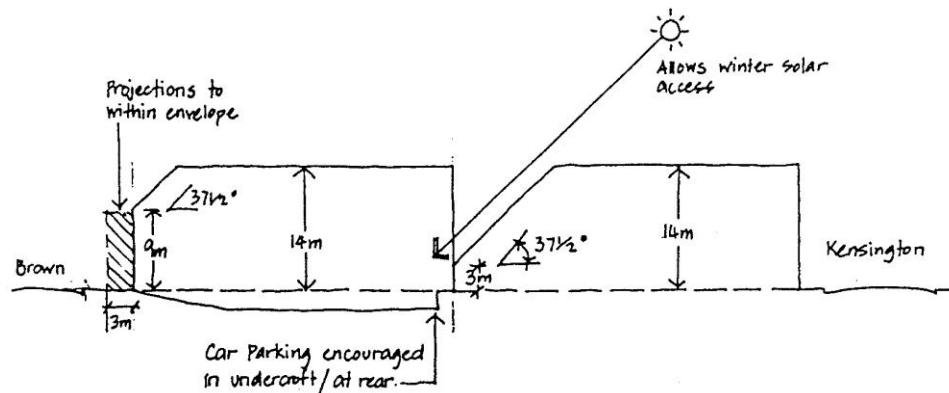


Figure 3:- Building Envelope.

3.3 Parking and Access

Undercroft or basement parking concealed from public view is preferred, although open air car parking at the rear of sites will be considered.

At-grade pedestrian access for all users into buildings is to be provided from Street frontages. Safe and attractive pedestrian access into buildings is also to be provided from any car park on site. Generally, one crossover only will be allowed for each lot.

3.4 Building Character

Buildings should respect the scale of the street or place and of their neighbours. The buildings should also be sympathetic in scale, massing and proportions to adjacent buildings and those of heritage significance. The opportunity exists to create a strong sense of identity and an innovative inner city style of residential, commercial and mixed use development with interesting facade treatments and roof lines.

Buildings will be assessed for their contribution to the legibility and identity of East Perth, the public-private interface, streetscapes, grain and sense of place. Aspects such as roof line, materials and the scale and massing of buildings will be carefully considered by the local government in the assessment of development applications.

The use of courtyards and/or balconies is encouraged to provide amenity for residential development where required, and to take advantage of any views. All elevations are to be well detailed and are to relate to a pedestrian scale.

3.5 Roofs

A range of roof forms/profiles which contribute to the identity of the area and which reflect the range of uses and development types in the precinct are possible. Roofs should generally be expressed in a way which complements the architectural style of the building, which provides clear silhouettes and minimises visual clutter. Roofs may be flat or, if pitched, between 30° and 45°.

3.6 Walls and Windows

Blank walls abutting a public space must be minimised. Walls are to be detailed to provide visual interest and pedestrian scale. Proportions of solid wall to openings should reflect the scale of adjacent spaces.

Windows are to have a vertical or square proportion. Where the overall window opening shape is of a horizontal nature, glazing shall be broken up to reflect the required square or vertical proportion. Reflective or dark tinted glass is not permitted.